

Traffic Considerations for Development Projects

Daniel R. Blechschmidt, P.E.
City of Columbus
Department of Public Service
Division of Traffic Management



Topic Overview

- ADA curb ramp policies
- ADA parking requirements
- Parking updates
- Signage in the right-of-way
- Traffic study processes and procedures
- Scoping of right-of-way improvements

ADA Curb Ramp Policies

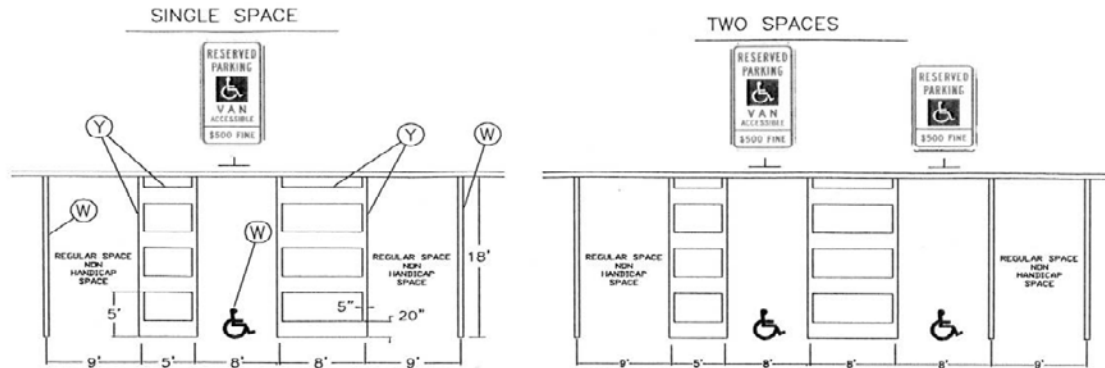
- Updated ADA Rules and Regulations for the Department of Public Service are in place with an effective date of April 1, 2018.
- These rules and regulations replaced the previous Wheelchair Ramp Requirements rules and regulations with an effective date of March 5, 2011.
- A major change to these requirements for a private development site are the design requirements at an intersection.

ADA Parking Requirements

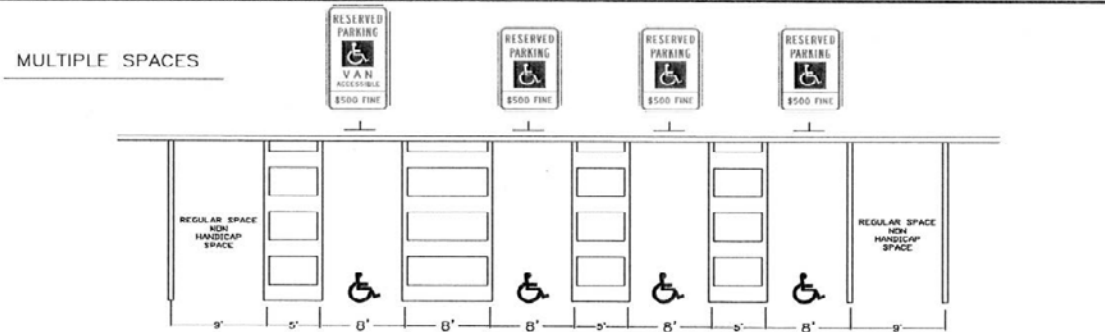
- The Department of Public Service maintains an ADA parking space striping policy which exceeds the minimum Federal standards, as linked here: <https://www.columbus.gov/publicservice/Design-and-Construction/document-library/ADA-Parking-Details/>
- All ADA parking spaces are required to have a minimum 5' access aisle on the driver's side of the parking space with appropriate numbers of van accessible parking spaces with an 8' access aisle on the passenger side of this parking space.
- The intent is to provide a better level of service for users.
- Exceptions to this policy may be considered on a case-by-case basis for parking lots with less than 10 spaces or non-public (i.e., enclosed residential) parking areas if a significant hardship is demonstrated by the applicant.

ACCESSIBLE PARKING SPACE SPECIFICATIONS

CITY OF COLUMBUS, OHIO DEPARTMENT OF PUBLIC SERVICE



SIGNS SHALL BE MOUNTED AT A MINIMUM OF 7' FROM GROUND TO BOTTOM OF SIGN



THE ABOVE TYPICALS ARE POLICY FOR THE DEPARTMENT OF PUBLIC SERVICE AND MEET OR EXCEED A.D.A. REQUIREMENTS

A.D.A. REQUIREMENTS

1. ACCESSIBLE SPACES SHALL BE LOCATED AS CLOSE AS POSSIBLE TO THE ENTRANCE OF THE FACILITY BEING SERVED.
2. ONE SIGN SHALL BE PLACED AT THE HEAD OF EACH ACCESSIBLE PARKING SPACE.
3. ONE IN EVERY SIX ACCESSIBLE SPACES, BUT NOT LESS THAN ONE, SHALL HAVE AN ACCESS AISLE 8' WIDE AND SHALL BE SIGNED "VAN ACCESSIBLE".
4. AT OUTPATIENT MEDICAL UNITS AND TREATMENT FACILITIES, 10% OF THE TOTAL NUMBER OF SPACES PROVIDED SERVING THE OUTPATIENT UNIT SHALL BE ACCESSIBLE.
5. AT UNITS OR FACILITIES THAT SPECIALIZE IN TREATMENTS OR SERVICES FOR PERSONS WITH MOBILITY IMPAIRMENTS, 20% OF THE TOTAL NUMBER OF SPACES SHALL BE ACCESSIBLE.

TOTAL NO. OF SPACES IN LOT	MINIMUM NO. OF ACCESSIBLE SPACES
1 TO 25	1
26 TO 50	2
51 TO 75	3
76 TO 100	4
101 TO 150	5
151 TO 200	6
201 TO 300	7
301 TO 400	8
401 TO 500	9
501 TO 1000	2% OF TOTAL
1001 AND OVER	20 PLUS 1 FOR EACH 100 OVER 1000

Parking Information

- Effective April 26, 2017, the Short North Special Parking Area went into effect, which created new parking requirements and new methods to manage parking variance requests.
- Effective January 13, 2017, the Department of Public Service has an established policy for situations where metered parking spaces would be proposed to be removed.
- Any specific questions on these items may be directed to Amanda Ford at (614) 645-6460 or aaford@columbus.gov.

Signage in Right-of-Way

- The Department of Public Service has a policy for signage changes that may be required in the right-of-way as a result of private development projects.
- This new policy only applies if there are signage changes triggered for a site development that does not require a E-Plan.

Signage Policy Procedures

- As a part of the site compliance plan review process, the Division of Traffic Management will determine if any signage changes are required in conjunction with a site development.
- The site compliance plan will be the reference document for signage changes in the right-of-way when this policy is triggered.
- All signage shall be placed in the right-of-way.

Signage Installation Details

- If this policy is triggered, the following elements will need to be shown on the site compliance plan:
 - All existing trees, landscaping, signage, utility poles and any other elements within 100' that may conflict with the proposed signage.
 - Location and orientation of proposed signage as well as the text and/or symbols contained on the proposed signage.
 - References to standard sign codes from the Ohio Manual of Uniform Traffic Control Devices or City of Columbus sign codes, where applicable.
 - A standard [note](#) will need to be added with developer information and City contact information.

Installation Responsibilities

- The developer will be responsible notifying the City of Columbus a minimum of 45 days in advance of the necessary signage installation.
- The City of Columbus will be responsible for the installation of these signs in the right-of-way.
- The developer will be responsible for the costs of this signage installation.
- A standard fee of \$350.00 per sign will be required to cover labor and material costs.
- This fee will need to be submitted prior to the release of the final site compliance plan.

Traffic Study Types – Impact vs. Access

- A traffic impact study is a study performed to study off-site intersections in addition to the access points for a proposed development.
 - A traffic impact study is required when a development meets certain trip thresholds (currently ≥ 200 or ≥ 400 peak hour trips) defined in the Traffic Standards Code (Section 4309 of City Code) upon submittal of a rezoning, zoning variance, special permit or preliminary subdivision plat application.

Impact Study vs. Access Study

- A traffic access study is a study performed to only study the access point(s) for a proposed development.
 - A traffic access study may be required in conjunction with a site compliance plan or upon submittal of a rezoning, zoning variance, special permit or preliminary subdivision plat application when access is taken from an arterial or collector street.
 - The scope of this study could include analysis of off-site intersection(s), but only to determine queuing impacts and/or turning movement limitations that may be necessary at the site access point(s).

Initiation of Study

- The traffic impact/access study will need to be performed under the supervision of registered professional engineer with experience in traffic engineering.
- Prior to commencing study preparation, it is strongly recommended to contact our office to discuss the necessary limits and parameters for a study.

Initiation of Study

- For any traffic impact/access study, a written MOU (Memorandum of Understanding) is a helpful means to ensure that the scope of study and the necessary analyses are appropriate.
- Prior to initiating a traffic impact study preparation or any sizable development proposal, an initial meeting to discuss the parameters for the MOU can be a helpful step.

Review Process

- Subject to workload and staffing levels, our goal is to complete traffic study reviews within a 30 day period to provide predictable timelines for the development community.
- Depending on the quality of the submittal, a traffic impact study would generally require 2 to 4 submittals and a traffic access study would generally require 1 to 3 submittals.
- Coordination with other jurisdictions may incorporate additional variables into the review process.

Approval of Study

- Upon review and approval of a traffic impact/access study, an approval letter is sent to the engineer responsible for the study.
- Approval of the traffic impact/access study will allow the scope of work for the right-of-way improvements associated with the development project to be established in order to develop the necessary engineered plans.

Future Considerations

- In conjunction with the Connect Columbus effort that is updating the Columbus Thoroughfare Plan, there are continuing efforts to establish new transportation impact study and access management guidelines.
- These efforts will include an update to the Traffic Standards Code, which has been in place in its current form since 1997.
- Additional information will be forthcoming.

Scoping Summary

- Since 2012, the Department of Public Service has provided scopes for right-of-way improvements being constructed in conjunction with private development projects.
- The Division of Traffic Management generates scopes for right-of-way improvements for private development projects with the Division of Design & Construction being responsible for the review of the applicable engineered plans.
- Upon review of a preliminary site compliance plan and/or final site compliance plan, the Division of Traffic Management determines if right-of-way improvements are required.
- The primary point of contact for scopes of work is Brandan Hayes, who can be reached at (614) 645-8354 or bmhayes@columbus.gov.

Scoping Process

- If right-of-way improvements are required, there may need to be coordination or items to be completed prior to a scope being generated that may include:
 - Preparation and review of a traffic impact/access study
 - Coordination due to right-of-way requirements of other City Divisions
 - Determining funding source of proposed improvements
 - Coordination with roadway improvements being constructed by the City as a part of a Capital Improvement Project
 - Submittal and review of a sidewalk construction exemption
 - Evaluation of field conditions
 - Evaluation of conceptual layout of roadway widening or non-standard design elements
 - Review of non-standard materials proposed in right-of-way
 - Coordination with City signal operations staff to determine impacts to adjacent signalized intersections
- When all coordination is complete, a scope for right-of-way improvements is generated and provided to the applicant and City plan review staff.

Plan Format

- The Division of Traffic Management initiates the appropriate plan format for improvements.
- There are three formats whereby right-of-way improvements may be performed:
 - E-Plan (Street Construction Plan)
 - CC-Plan (Stormwater Plan)
 - Right-of-Way Permit
- The type of right-of-way improvements that are being performed and the need for a CC-Plan dictate the appropriate plan format.

E-Plan Requirement

- An E-Plan is required to be prepared if any of the elements are required as part of the scope:
 - Roadway construction, widening, reconstruction, or mill and overlay of an existing roadway.
 - Signal work (includes pedestrian pushbuttons to maintain ADA compliance or detector loop relocations).
 - Signage and striping changes to the existing roadway.
 - Any roadway improvements where a limited access point is proposed (i.e., right-in/right-out, right-in).
 - Any modifications or installation of a median.
 - Non-standard sidewalk, curbing or curb ramp installation.

Improvements Shown on CC-Plan

- If the Division of Sewerage and Drainage determines that a CC-Plan (Stormwater) is required, but an E-Plan is not required, right-of-way improvements shall be shown on the associated CC-Plan.
- Right-of-way improvements shown on a CC-Plan are to be limited to:
 - Standard driveway approaches
 - Standard sidewalk or shared use path
 - Standard curb ramp installations
 - Standard curb installations (no substantial drainage impacts)

Right-of-Way Permit

- If an E-Plan or CC-Plan (Stormwater) is not required, right-of-way improvements will need to be installed by right-of-way permit.
- All right-of-way improvements will need to be defined and shown on the final site compliance plan.
- Upon approval of the final site compliance plan, a copy of this approved plan will need to be provided to the Department of Public Service Permit Office located on the 1st Floor of 111 N. Front St. prior to permit issuance and the installation of improvements.

Important Reminders

- A scope of right-of-way improvements cannot be issued if the review of a traffic impact/access study is still pending or if outstanding traffic impact/access study comments still need to be addressed.
- When roadway widening is planned, a conceptual layout will need to be reviewed and approved prior to the development of a scope of work and prior to E-Plan submittal.
- If right-of-way improvements are shown on a CC-Plan, these quantities need to be explicitly defined on Page 1 of the CC-Plan in the Estimate of Quantities.

Future Considerations

- Refuse Collection Standard Notes
 - There are discussions occurring related to a means to streamline the documentation of the intended refuse collection provider with the submittal of a final site compliance plan.
- Sidewalk and Bikeway Facility Requirements Rules and Regulations
 - There are discussions occurring related to updating and clarifying these requirements.

Questions

- Any questions?
- Additional questions related may be directed to my attention:

Daniel R. Blechschmidt, P.E.

Division of Traffic Management

Department of Public Service

Phone: (614) 645-1694

E-mail: drblechschmidt@columbus.gov